

TERMINAL ONE WHARF: EXISTING CONDTIONS



MILLER KNOX REGIONAL SHORELINE PARK
(FERRY POINT ZONE)

MILLER KNOX REGIONAL SHORELINE PARK
(RIDGE ZONE)

(BAY SHORE ZONE)

FERRY POINT
BEACH

DORNAN DRIVE

BRICKYARD COVE ROAD

SHORELINE DRIVE

SAN FRANCISCO BAY

TERMINAL ONE WHARF

RICHMOND
YACHT CLUB

50' 

- 1 Existing Bay Trail
- 2 Proposed Bay Trail
- 3 Proposed Ridge Trail connector
- 4 Bay Trail road crossing (raised to be flush with Trail)
- 5 Public on-street parking, TYP.
- 6 Trash collection & loading zone
- 7 Existing rip-rap revetment
- 8 Existing pump station
- 9 Speed table pedestrian crossing
- 10 Pedestrian Bridge & bay overlook
- 11 Play theater
- 12 Family picnic pavilion
- 13 Existing tie-off / viewing pier
- 14 Rail to trail pathway
- 15 Boardwalk gardens
- 16 Raised viewing deck
- 17 Entry Plaza
- 18 Kinetic, power-generating wind sculpture
- 19 Podium play space
- 20 Terminal One Mews: pedestrian / bike only with emergency access
- 21 Resident dog run
- 22 Stormwater treatment gardens
- 23 Podium terrace access
- 24 Garage access
- 25 Lawn with "Turf Block" for emergency access
- 26 Entry stoop & garden, TYP.
- 27 Native grass meadow on podium
- 28 Amenity pavilions (on podium)
- 29 Wayfinding environmental graphics
- 30 Private patio, TYP.
- 31 Flexible lawn
- 32 Hammock or porch swing grove





PROPOSED PUBLIC OPEN SPACE IMPROVEMENTS:

- - - - - TERMINAL ONE PROPERTY LINE
- - - - - PUBLIC SPACE IMPROVEMENT AREA OUTSIDE TERMINAL ONE PROPERTY LINE
- — — — — PUBLIC OPEN SPACE PARCELIZATION



PROPOSED PUBLIC OPEN SPACE IMPROVEMENTS WITHIN THE 100' SHORELINE BAND:

- - - - - TERMINAL ONE PROPERTY LIMIT
- - - - - BCDC 100' SHORELINE BAND



EXISTING AND PROPOSED BAY TRAIL CIRCULATION:

- - - - - (E) Bay Trail
- — — — — Bay Trail Extension
- ||||| Bay Trail Extension Crossing
- Pedestrian and Bicycle Priority Mews
- - - - - Locations of 15' wide Bay Trail with guardrail and 36" shoulder along roadway
- Bicycle Node: Locations to provide seating, wayfinding signage, and/or bicycle parking.

PROJECT AREA CALCULATIONS:

TERMINAL ONE PROPERTY TOTAL AREA: 13.12 ACRES
 PUBLIC OPEN SPACE: 5.55 ACRES (42% OF TOTAL AREA)
 HOUSING + COMMUNITY OPEN SPACE: 7.57 ACRES



AREA: APPROXIMATELY 1.13 ACRES



WHARF PARK OUTDOOR ROOMS WITHIN THE WAREHOUSE FOOTPRINT



20'

FEATURES:

- 1. Elevated picnic deck
- 2. Overlook deck
- 3. Wharf Promenade with fishing access
- 4. Play theater
- 5. Hammock grove
- 6. Resurfaced pier deck
- 7. Picnic pavilion
- 8. Existing dock cleat-preserved in place
- 9. Bay Trail Shoreline extension
- 10. Bike parking & wayfinding graphics
- 11. (E) Wharf warehouse foundation walls
- 12. Lawn
- 13. Raised planters
- 14. Stormwater treatment area
- 15. ADA Parking
- 16. Re-use wharf pediment sign at park entry
- 17. Bridge connecting Bay Trail to Wharf Park
- 18. Raised table-crossing
- 19. Terminal One Mews entry plaza



ILLUSTRATIVE VIEWS



View A: West entry of the wharf includes re-use of the existing warehouse pediment sign.



View B: Foundation walls of the existing warehouse frame a group picnic area. The raised deck allows visitors unobstructed panoramic views to the Bay Bridge, San Francisco, Angel Island, Mount Tamalpais, Ferry Point and the Richmond Bridge.



View C: Entry from the center of the wharf into the Play Theater with stepped boulders for climbing and seating, surrounded by a flexible lawn for play and events.



View D: East entry of the wharf looking south with a bridge connection from the Bay Trail to the Wharf Park and an entry garden of colorful succulents in raised planters with built-in overlooks.



View E: Wharf Promenade looking west towards Mount Tamalpais. The foundation walls of the existing warehouse outline the park spaces and help retain soil for raised lawn, gardens, and viewing decks. The Picnic Deck and Hammock Grove can be seen in the background. The grove offers wind protection and shade for the lawn area.



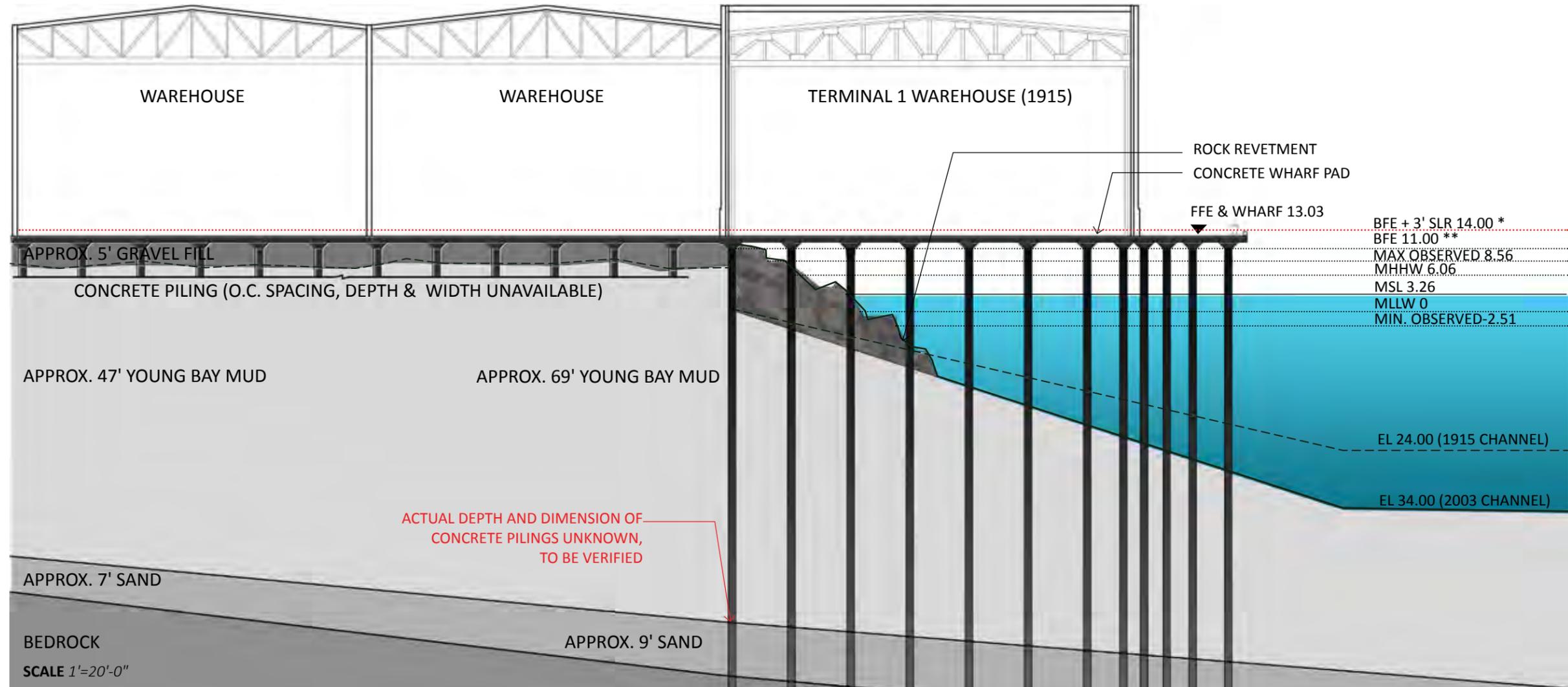
DISCLAIMER: The project applicant is in the process of performing a structural engineering analysis of the existing wharf and pilings. This exhibit is diagrammatic and conceptual in character. It was prepared by overlaying the 1912 construction drawings of the wharf on to the 2005 Soil Risk Management Plan prepared by Geomatrix Consultants for the City of Richmond Redevelopment Agency. The overlay is to-scale; however, further investigation is necessary to determine the 1915 as-built conditions relative to the 1912 construction drawings, to include consideration of any engineering retrofits that may have been undertaken during the post-1915 period, and to further assess the geotechnical conditions underlying and in proximity to the wharf.

Document Sources:
 EVALUATION OF TERMINAL ONE - Base Flood Elevation and Inundation Area: Prepared for the Briscoe Ivester & Bazel LLP, January 1 2015 (Job No. 20145024-10) by BKF Civil Engineers.

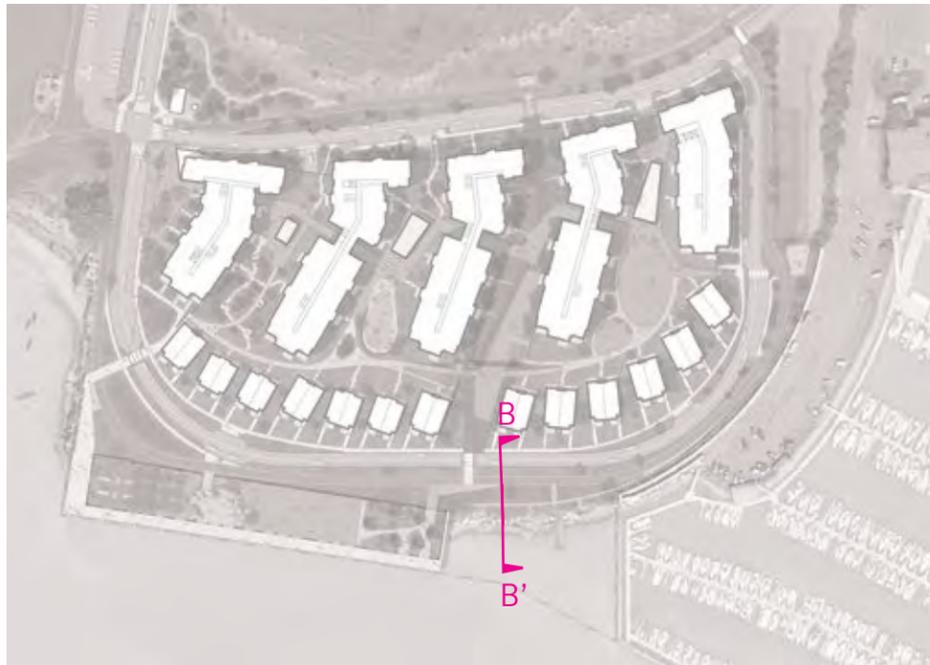
SOIL RISK MANAGEMENT PLAN - Terminal One Site 1500 Dornan Dr. Richmond CA: Prepared for the City of Richmond Redevelopment Agency, February 2005 (Project No. 6293.002) by Geomatrix Consultants.

RICHMOND HARBOR PROJECT - General Design Outer Harbor Wharf No. 1, Plate 6: Prepared for the City of Richmond, September 1912 by Haviland & Tibbetts Civil Engineers.

NOTES:
 1. All Elevations are based on NAVD 88.
 2. NOAA, Station 941863 Richmond, CA
 * Terminal One Project, Draft E.I.R., ESA/140325 Feb. 2016, P.2-23
 ** Base Flood Elevation and Areas of Inundation Memo, BKF 01.15.2015



SECTION A-A': EXISTING WHARF



Sea Level Definitions:

BFE - Base Flood Elevation: The computed elevation to which floodwater is anticipated to rise during the base flood. Base Flood Elevations (BFEs) are shown on Flood Insurance Rate Maps (FIRMs) and on the flood profiles.¹

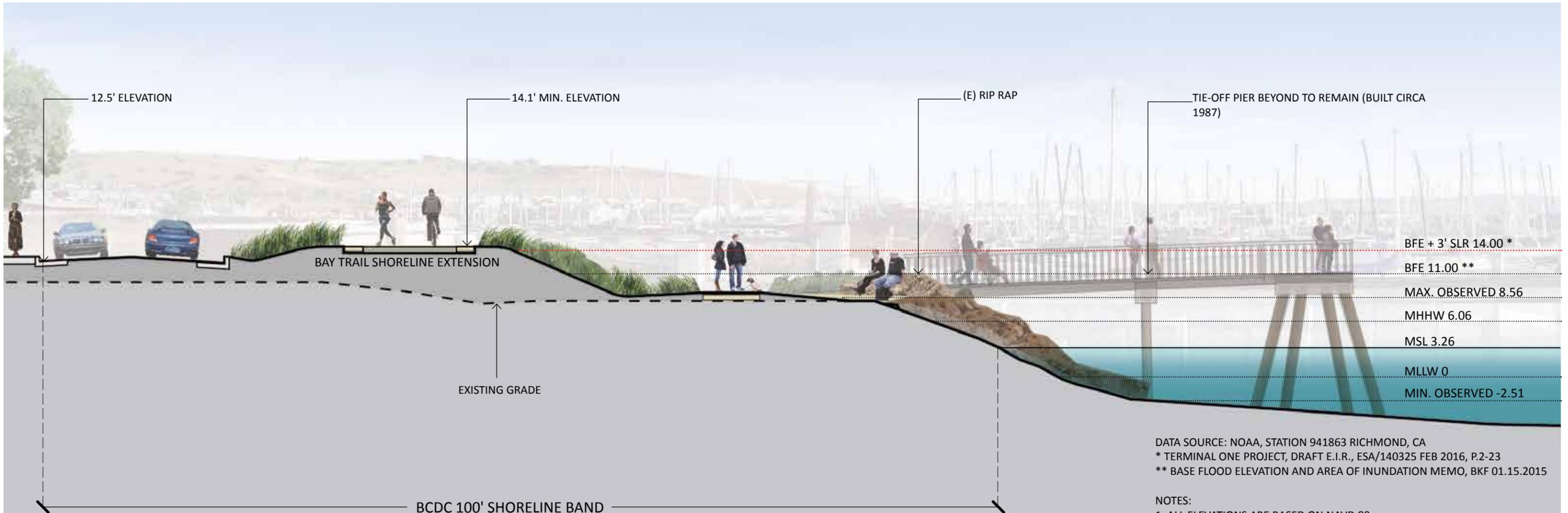
MHHW - Mean High High Water: The average height of the higher high waters over a 19-year period. For shorter periods of observation, corrections are applied to eliminate known variations and reduce the result to the equivalent of a mean 19-year value.²

MSL - Mean Sea Level: The average height of the surface of the sea for all stages of the tide over a 19-year period, usually determined from hourly height readings.²

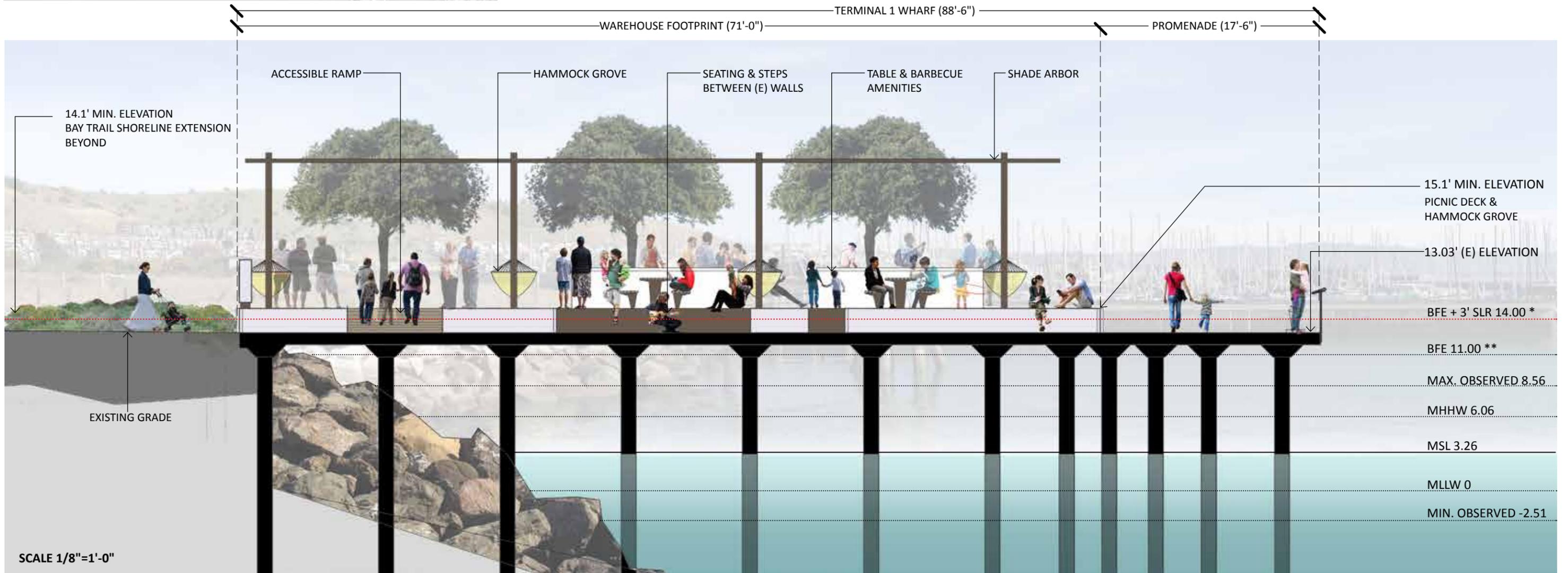
MLLW - Mean Low Low Water: The average height of the lower low waters over a 19-year period. For shorter periods of observations, corrections are applied to eliminate known variations and reduce the results to the equivalent of a mean 19-year value. Frequently abbreviated to Lower Low Water.²

¹Definition provided by FEMA <http://www.fema.gov/base-flood-elevation>

²Definition provided by NOAA <http://shoreline.noaa.gov/glossary.html>

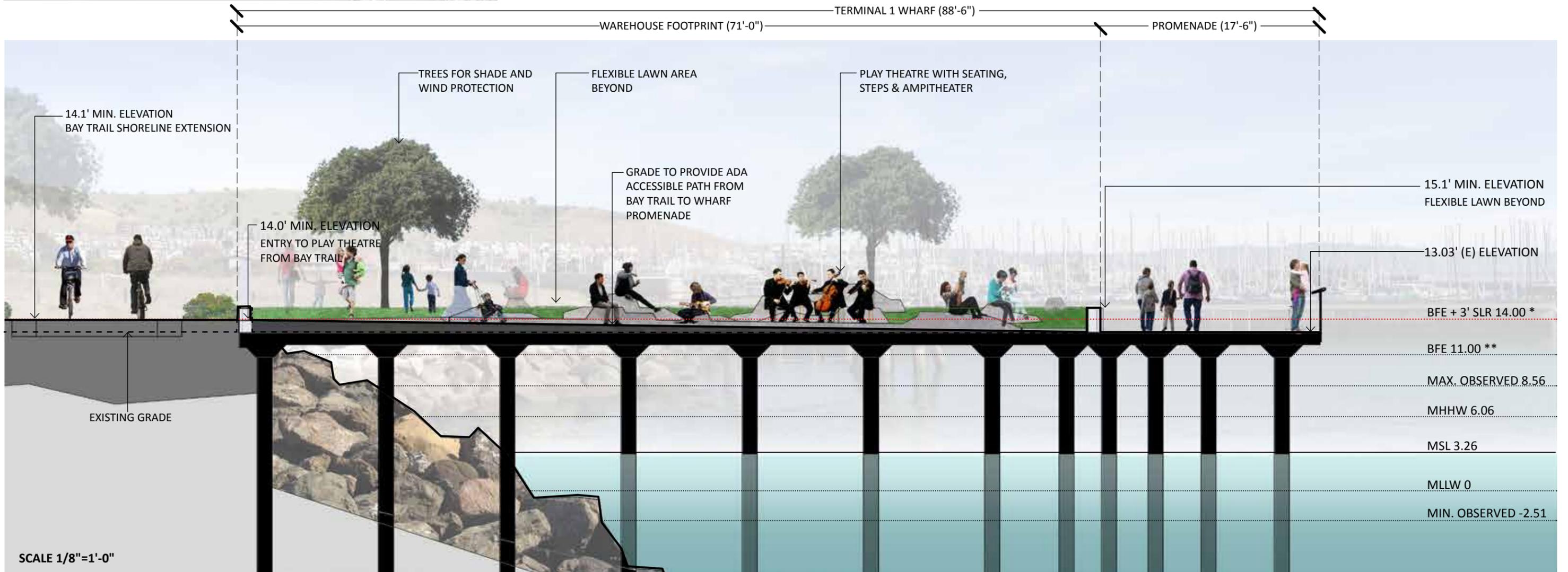
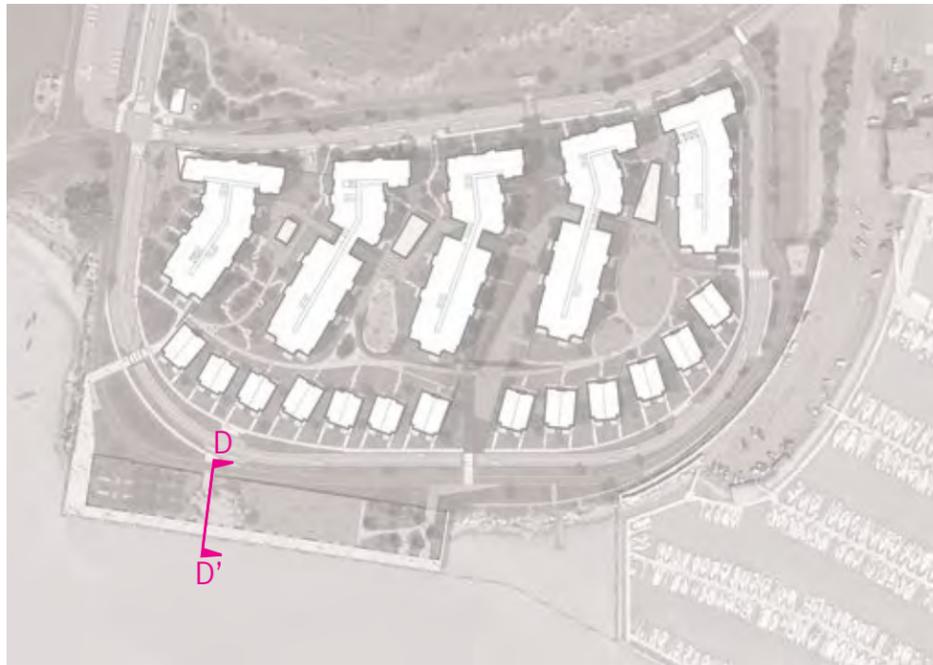


SCALE 3/32"=1'-0"

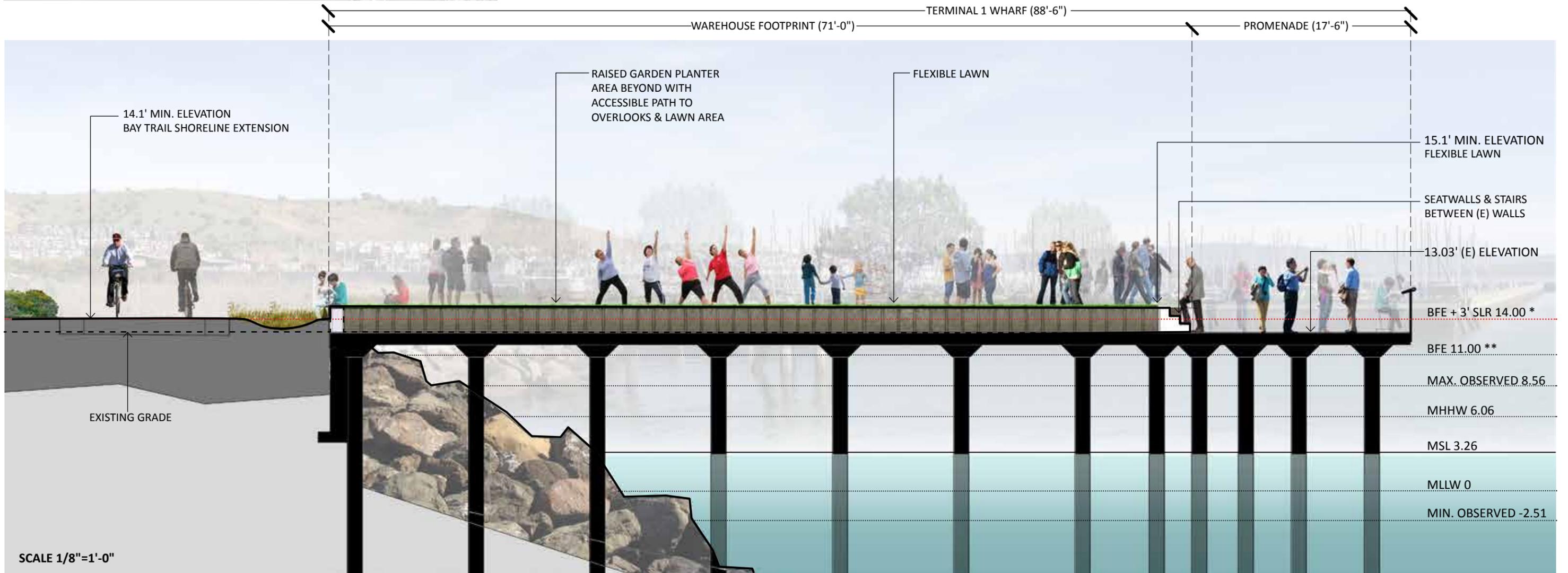
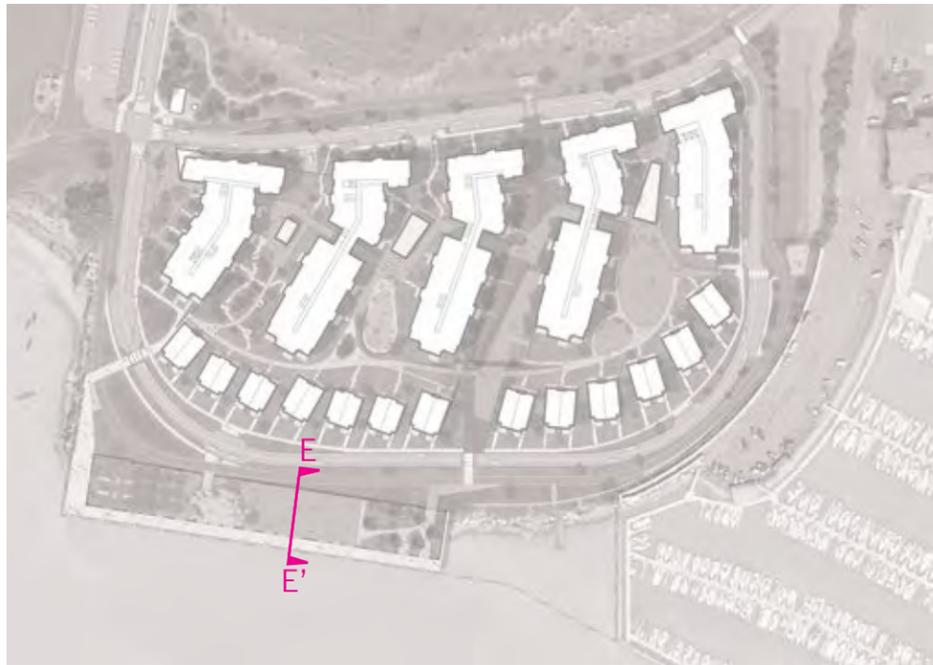


SCALE 1/8"=1'-0"

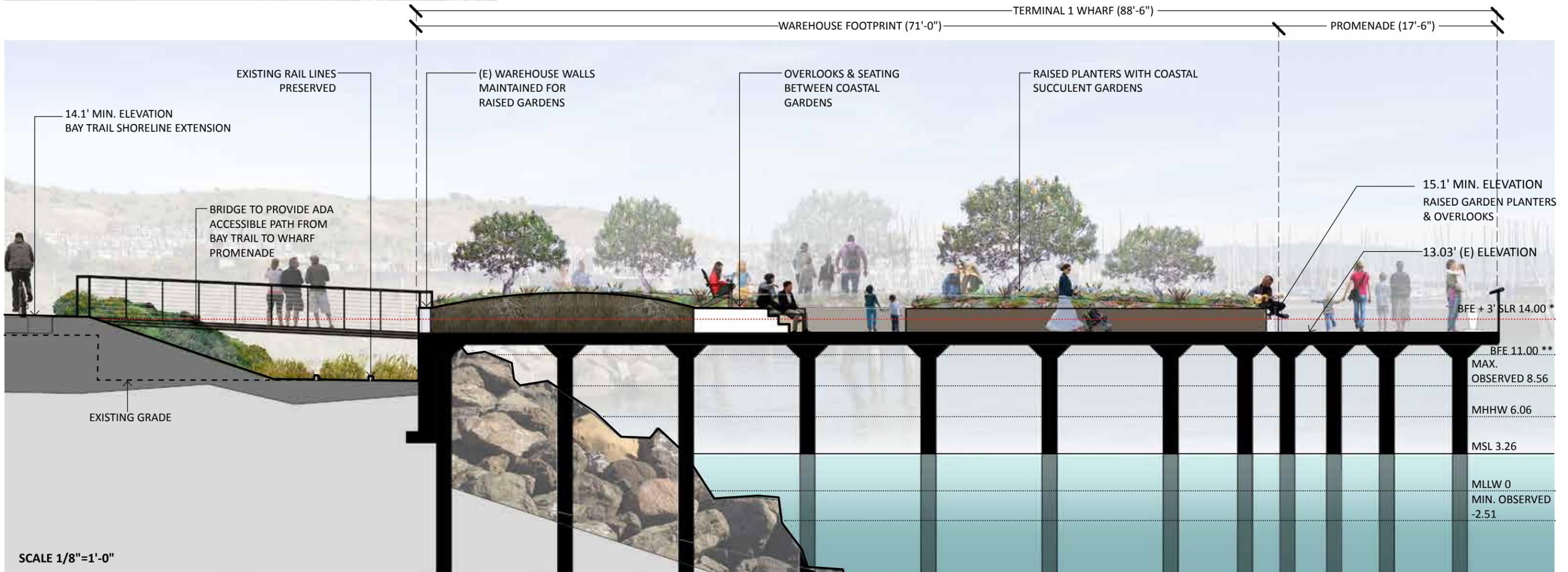
SECTION C-C': PROPOSED PICNIC DECK & HAMMOCK GROVE, Work in Progress



SECTION D-D': PROPOSED PLAY THEATER, Work in Progress



SECTION E-E': PROPOSED RAISED FLEXIBLE LAWN, Work in Progress



SCALE 1/8"=1'-0"

SECTION F-F': PROPOSED COASTAL GARDENS, Work in Progress